

WARD: Bedminster

SITE ADDRESS: 155 - 165 West Street Bedminster Bristol BS3 3PN

APPLICATION NO: 22/00805/F Full Planning

DETERMINATION DEADLINE: 19 September 2022

Demolition of existing buildings and construction of student residential accommodation (12 studios flats and 7 cluster flats) and a replacement employment unit (Use Class E(g)), and associated works. (Major application)

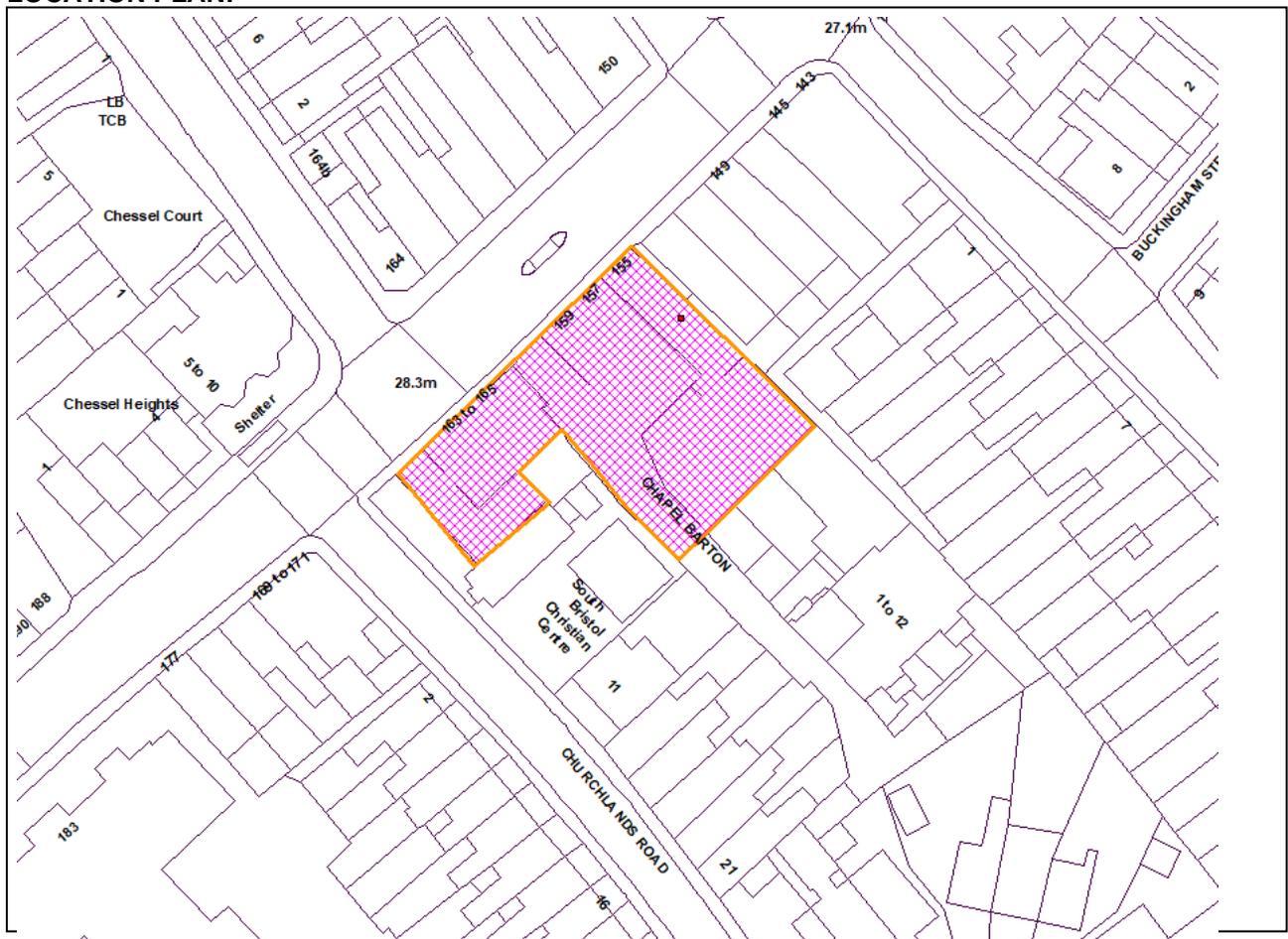
RECOMMENDATION: GRANT subject to Planning Agreement

AGENT: CSJ Planning Consultants
1 Host Street
Bristol
BS1 5BU

APPLICANT: Paragon Bristol Developments
C/O CSJ Planning Consultants
1 Host Street
Bristol
BS1 5BU
United Kingdom

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



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REASON FOR REFERRAL

In response to the significant public interest the application has received, it is referred to Committee for determination.

SUMMARY

Planning permission is sought to demolish existing buildings at the site, to facilitate the construction of student accommodation (19no. flats: 12no. studios, 7no. cluster flats) and a replacement employment unit Use Class E(g). The existing site is composed of a vehicle repair workshop and a car showroom, which is split by a private access road to Chapel Barton flats.

The development has been questioned on the grounds of the proposal being located outside of the city centre, in a location perceived by some to be far away from higher education establishments. Although planning policy generally supports specialist student housing in the city centre, it does not prohibit student housing proposals elsewhere in Bristol, provided they meet the 'general criteria' set out in policy DM2. The application complies with the general criteria of policy DM2, as it would not harm the amenity and character of the locality, and nor would it create or contribute to a harmful concentration of student uses within the locality. The redevelopment of the site is encouraged by policy and guidance, as it represents a brownfield site in an accessible location close to good public transport routes, including regular bus services to places students would likely want to go.

To the proposal's credit, it has been demonstrated that it meets the expectations of policy BCS18, as it would help to: diversify the housing stock in the area, address the demand for specialist student accommodation, and adapt to a changing population where student numbers in Bristol are expected to continue to rise. The proposal would also reduce pressure on existing housing stock, for example through conversions of family homes to HMOs. Substantial weight must be attributed to proposal's contribution to Bristol's housing supply, which as well as helping to provide needed student accommodation, also counts towards the Council's five year housing land supply.

Neighbours have understandably expressed concerns that the proposal will result in unacceptable levels of on-street car parking in the locality. In response, Transport Development Management has reviewed the application, including transport statements and a parking survey, and confirms that there is sufficient capacity in the locality to safely accommodate any car parking associated with this development. The proposal is not expected to unacceptably impact neighbours through noise and disturbance. A management plan has also been provided, which provides further reassurances. The development includes sustainable design measures and provides an acceptable living environment for future residents.

In the planning balance, there is a presumption in favour of granting planning permission, unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits. This report demonstrates there are substantial benefits that would flow from this development, against which there are no significant or demonstrable harms associated with approving this development. The application is therefore recommended for approval, subject to a s.106 Agreement and conditions.

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The application site is known as 155 – 165 West Street in Bedminster. The existing site is composed of a vacant vehicle repair workshop (Use Class B2) (no. 155) and a car showroom (sui generis) (nos. 157 – 159), which is split by a private access road to Chapel Barton flats and the rears of Brighton Terrace. The site is bound to the north east by the rear gardens of Bartlett's Road, to the north by properties fronting West Street, and to the south by the South Bristol Christian Centre.

The site is not within a Conservation Area or the setting of a heritage asset. The section of West Street adjacent to the site is not a designated shopping area or frontage, and the site is not within a designated centre, the nearest being Bedminster Town Centre and Primary Shopping Area (East Street) - approximately 600 metres walk from the site. West Street is a safeguarded transport link (Hengrove to the City Centre), and the site is within an Air Quality Management Area. The site is within a South Bristol Article 4, which removed permitted development rights for changes of use from C3 to C4.

APPLICATION

Full planning permission is sought to demolish all the existing buildings at the site in order to facilitate the construction of purpose built student accommodation (PBSA) and a replacement employment unit Use Class E(g). Due to the amount of proposed floorspace, the application is a major planning application.

The PBSA includes 19no. flats, of which 12 are studios and 7 are cluster flats, which provides 49 bed spaces in total, with 37 of these in cluster flats. The cluster flats are composed of the following bedroom types: 2no. 4beds, 1no. 5 bed, and 4no. 6 beds.

The employment unit includes approximately 82 sq.m of floorspace, and in accordance with Use Class E(g) can be used for the following purposes: an office, a research and development facility, and industrial processes that can be carried out in a residential area without causing detriment to the amenity of the area.

The proposal forms two blocks, both of which address West Street at a 2.5 to 3 storey scale and stretch to the rear at 2.5 to 3 storeys. Block A is located on the north-eastern side of the site and hosts the employment unit that addresses West Street, with student accommodation and garden space to the rear. Block B is located on the north-western side of the site and includes student accommodation, with a lobby space and other ancillary uses adjacent to Chapel Barton and West Street.

- Two car parking spaces are proposed on site, including a wheelchair accessible space. Further, a car club space is proposed to be provided in the locality;
- 19no. cycle parking spaces are proposed within a ground floor store in Block A, accessed from Chapel Barton;
- A refuse store is proposed within Block B on the ground floor, with direct access onto West Street;
- A lobby is proposed on the ground floor of Block B on the western side of Chapel Barton facing West Street, which includes a reception area for the management staff, post/parcel boxes, and TV/lounge area for residents;
- Two garden areas are proposed to the rears of both blocks of accommodation;
- Access to the Chapel Barton flats will not be materially restricted by the development.

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EVOLUTION OF APPLICATION

Over the course of the application the applicant has worked to address concerns raised by officers, and others, and has submitted revised plans, as well as a further transport statement, and other reports. Further neighbour notification was issued following the receipt of such plans. The revised plans largely addressed concerns raised by officers, including the impact of the proposal on neighbours; the proposal's appearance and impact on the character of the area; and the layout of the proposal at ground floor level.

RELEVANT PLANNING HISTORY

155 West Street

12/02213/F - Increase height of the roof to create space for additional storage and height on ground floor for a 4 post ramp. - Refused due to design and sustainability

163 - 165 West St

05/01803/F - Construction of single storey office extension to rear.- Granted 2005

157-159 West Street

11/02190/F - Retrospective change of use of premises for car storage, car sales, offices and associated external alterations. - Granted 2011

153 West St

08/04346/F – Change of ground floor from shop (Use Class A1) to 1 no. residential unit with alteration to the front elevation. - Granted 2008

1 Bartletts Rd

19/01459/CP - Application for a Lawful Development Certificate for a Proposed use or development - Proposed loft conversion with rear dormer roof extension and 2no roof lights to front elevation. New patio door and hallway window to rear elevation and replacement SVP. – Issued (approved) 2019

COMMUNITY INVOLVEMENT

Avril Baker Consultancy prepared a Report of Community Involvement, which sets out the community involvement the applicant engaged in prior to submitting this application. Stakeholders were notified by letter and email, which included an overview of the proposal and a link to website to enable feedback. Ward councillors were offered a briefing, which resulted in a meeting/site visit. Details of the application were also shared in posts on Southville, Ashton and Bedminster Information Board, BS3 Connect, and WHaM (Windmill Hill and Malago) and Way Out West. The Bristol Post also published an article regarding the development.

Comments received in response to the development concerned three main themes, largely matching those submitted as comments during the application: insufficient car parking in an area that suffers from existing car parking issues; concerns regarding the type of development for students; and concerns regarding antisocial behaviour, including refuse and noise. Concerns were also expressed

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regarding overlooking/privacy, overcrowding, levels of light, and construction. Page 9 – 11 sets out concerns expressed by stakeholder in detail, along with the Applicant's Team Response.

The breadth of community engagement is proportionate to the scale and nature of the development. Overall, it is evident that the application has tried to address the concerns raised in a positive manner, albeit not to the degree that some stakeholders envisaged.

RESPONSE TO PUBLICITY – MEMBERS OF THE PUBLIC

The application was advertised by site and press notice, and neighbours were notified of the application by letter. Further notification occurred following the submission of revised plans. In response to this publicity and notification, 21 objections and 3 support comments were received from a total of 14 addresses.

Principle:

- There is a need for houses for families and young professionals, not students;
- Universities are in the centre and north, not the south;
- The development is out of scale with the neighbourhood;
- Insufficient services in the area to support the development;
- The plans should be rejected in favour of affordable housing;
- Student housing is not appropriate for the area, suggestion that students will not contribute to the community;
- Volume of the development is excessive;
- Support for the development as it will increase business on West Street

Urban Design

- Insufficient planting;
- Acknowledgement that the revised design improves the development's appearance;
- Support for the style of architecture.

Impact on neighbours:

- Noise and other activities from the development will impact detrimentally neighbours.

Amenity of Future Residents:

- The proposal suggests future residents will be mistreated.

Highway Safety and Transport:

- Concerns that no car ownership will not be able to be enforced and monitored;
- Existing car parking pressures in the area;
- Illegal / poor car parking could result in difficulties for disabled residents;
- Insufficient refuse storage, will result in a mess on the highway;
- Parking survey should consider Bristol, not Bath;
- The area needs an RPZ;

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- Insufficient cycle storage;
- The Transport Statement includes many inaccuracies.

Windmill Hill & Malago Community Planning Group – support comment:

- The height and building form is consistent with the local neighbourhood, as is the choice of materials and finishes;
- Concerns with regard to the number of flats.

West Street Neighbourhood Group, Way Out West – objection comment:

- The location is unsuitable for student accommodation, this is a long-established family area;
- The development will result in late night noise and parking issues.

RESPONSE TO PUBLICITY – STATUTORY AND NON-STATUTORY CONTRIBUTORS

Transport Development Management (BCC) has commented as follows:-

No objection, subject to conditions and a financial contribution being secured by s.106 Agreement to secure a car club space in the highway (TRO).

- Highway safety: the principle of the development is acceptable.
- Car parking: Given the submission of a parking survey, which demonstrates there is parking availability within the area, as well as the introduction of a car club space, Transport Development Management raise no further concerns regarding parking accommodation.
- Storage: sufficient cycle and refuse storage is proposed.
- Construction management: a construction management plan should be secured by condition.

Urban Design Team (BCC) has commented as follows:-

No objection.

Sustainable City Team (BCC) has commented as follows:-

No objection subject to conditions.

- The proposed gas boilers are now replaced with ASHP system for the full heating and hot water load - this is compliant with the heat hierarchy and is supported.
- Heat pumps plus PV array achieve a 38.4% reduction in emissions through renewables - this exceeds policy requirements
- An overheating assessment has been provided covering current and future weather scenarios
- All assessed spaces are compliant with both CIBSE TM52 and CIBSE TM59 for the current weather file
- For 2050 risk can be mitigated with increased natural ventilation - this is acceptable.
- For 2080 risk is present but potential mitigations are proposed including increased mechanical ventilation and replacement glazing - this is acceptable.

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Air Quality Team (BCC) has commented as follows:-

No objection, the Team do not have any concerns about air quality for this development.

Pollution Control (BCC) has commented as follows:-

No objection to the proposal subject to conditions.

- If the ground floor employment use is not restricted to E(g)(i), conditions are required to ensure acceptability, such as noise controls.
- Further conditions are required to ensure noise levels from air source heat pumps do not harm neighbours, and a construction environmental management plan is required.

Arboricultural Team (BCC) has commented as follows:-

No objection, subject to conditions.

Nature Conservation Officer (BCC) has commented as follows:-

The Preliminary Ecology Assessment for this proposal is acceptable and informs the application of any nature conservation issues. The site is located in an urbanised area and is not within or directly adjacent to any designated wildlife sites. There is no objection from Nature Conservation for this application, with the following conditions.

- A condition to secure a soft landscape plan,
- An ecological mitigation and enhancement strategy,
- To secure living roofs within the development.

The Public Protection (Land Contamination) Team (BCC) has commented as follows:-

The proposed development is sensitive to contamination and is situated on or adjacent to land which has been subject to land uses which could be a potential source of contamination. The submitted Desk Study is generally acceptable although we have a number of detailed comments about land uses on site which will be provided via email. This must be provided to their environmental consultants before any site investigation takes place. As the site was subject to bombing in World War Two the applicants must undertake a unexploded ordnance risk assessment prior to intrusive investigation or construction. We welcome submission of any additional information prior to determination, if not available standard planning conditions are recommended.

Lead Local Flood Authority (BCC) has commented as follows:-

Concerns expressed in relation to the drainage proposals, largely as it is a pump-based system and not a gravity-based system. A condition has been agreed.

Crime Reduction Unit has commented as follows:-

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No objection subject to conditions. Concerns expressed to final build details in relation to: access control/visitor entry, window and door design, mail and parcel security, lighting to external areas, and CCTV coverage.

The Conservation Advisory Panel has commented as follows:-

Final Comments:

This is a simple application in design terms. Whilst the render is accepted, anti graffiti measures need to be incorporated. This should be conditioned. Keep an eye out for heritage walls on and adjacent Chapel Barton, including older foundations inside this site boundary.

Original Comments:

The Panel welcomes the replacement of the single storey building by buildings of a more appropriate scale which repair the lost streetscape.

The deep panels above the first floor windows together with the mansard style roof would give a top heavy appearance, which could be improved by the top floor being in the form of set-back pavilions.

Bristol Waste has commented as follows:-

Support for the position of the bin store. In a general manner, the comments also reiterated their expectations for the number of waste receptacles needed to be housed within the store.

RELEVANT POLICIES AND GUIDANCE

- National Planning Policy Framework, 2021 - referred to hereafter as “**NPPF**”
- Planning Practice Guidance - referred to hereafter as “**PPG**”
- Bristol Core Strategy (Adopted June 2011) – referred to hereafter as “**CS**”, policies starting with “**BCS**” are from this plan
- Site Allocations and Development Management Policies (Adopted July 2014) – referred to hereafter as “**SADMP**”, policies starting with “**DM**” are from this plan
- SPD – Planning Obligations (Adopted September 2012)
- SPD – Urban Living (Adopted November 2018) – referred to as “**UL SPD**”
- SPD7 – Archaeology and Development (Adopted March 2006)
- PAN15 – Responding to Local Character – A Design Guide
- HMO SPD

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

For information, any policy containing BCS is from the Core Strategy, and DM is from the Site Allocation and Development Management Plan. Further, the remaining report uses the terms PBSA, specialist student accommodation and student housing interchangeably.

(A) Principle of Development

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i. Purpose Built Student Accommodation – Key Policies

The key policy for PBSA is policy DM2, which aims to ensure that specialist student housing proposals preserve the residential amenity and character of an area, and that harmful concentrations do not arise. To achieve this, the policy has two key limbs of assessment (referred to as the 'general criteria'), stating that development will not be permitted where:

(i). The development would harm the residential amenity or character of the locality as a result of any of the following:

- *Levels of activity that cause excessive noise and disturbance to residents; or*
- *Levels of on-street parking that cannot be reasonably accommodated or regulated through parking control measures; or*
- *Cumulative detrimental impact of physical alterations to buildings and structures; or*
- *Inadequate storage for recycling/refuse and cycles.*

(ii). The development would create or contribute to a harmful concentration of such uses within a locality as a result of any of the following:

- *Exacerbating existing harmful conditions including those listed at (i) above; or*
- *Reducing the choice of homes in the area by changing the housing mix.*

A key issue raised has been the proposal's location. Policy DM2 suggests in principle that specialist student housing will be acceptable within the city centre, but it does make clear that the policy does not prohibit student housing proposals outside of the city centre, specifically the policy states:

"Specialist student housing schemes will be acceptable within the city centre. Other locations may be suitable subject to the general criteria set out above".

The general criteria referred to in this quote is set out above, criteria i and ii of policy DM2.

Policy BCS18 should also be considered, as the policy expects developments to maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities. To achieve an appropriate tenure, type and size mix, policy BCS18 advises that the developments should aim to:

- *Address affordable housing need and housing demand;*
- *Contribute to the diversity of housing in the local area and help to redress any housing imbalance that exists;*
- *Respond to the requirements of a changing population;*
- *Employ imaginative design solutions*

Both policies BCS18 and DM2 expects development to provide a good standard of accommodation for future residents.

It is therefore clear that in principle, student accommodation is acceptable in this location, subject to the development meeting the expectations of policy DM2 and BCS18 in relation to its impact on the residential amenity and character of the locality (criteria i), and whether the proposal would create or contribute to a harmful concentration of student uses within the locality (criteria ii).

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ii. Criteria i of Policy DM2 – Impact on the residential amenity and character of the locality

The site is constrained due to its shape and proximity to many neighbouring residential buildings, as well as the South Bristol Christian Centre. Subject to details of management, travel planning and on-street parking, and suitable design solutions, the proposal is expected to meet criteria i of policy DM2 in principle. A detailed assessment of the proposal's impact on residential amenity and the character of the locality is included in the remaining report:

- Key Issue B considers the proposal's quality of design and standard of amenity of future residents;
- Key Issue C considers the proposal's impact on neighbours; and
- Key Issue D considers the proposal's impact on highway in the locality, including levels of on-street parking.

iii. Criteria ii of Policy DM2 – would the proposal create or contribute to a harmful concentration of student uses within the locality?

Policy DM2 advises that assessments should consider the relative impacts at street, neighbourhood and ward levels. While a significant PBSA development has been permitted to the north east in the Southville Ward (Dalby Av/Whitehouse Lane, ref. 20/05811/F), at street, neighbourhood, and ward level local to the site, Council records suggests there is limited PBSA, as do the records for the Bedminster Ward. Not all students live in PBSA, many live in HMOs, hence officers have also reviewed the number of HMOs in the area, finding there to be low numbers of multiple occupancy dwellings in the vicinity (HMOs as a percentage of housing stock: 0.93% of in the ward, and 2.09% within 100 metres of the site). Accordingly, the proposal would not create or contribute to a harmful concentration of student accommodation within the locality, due to reducing the choice of homes by changing the housing mix (policy DM2ii). Rather, the proposal would contribute to the mix and diversity of housing types in the area, as expected by policy BCS18.

Indeed, amongst other things, policy BCS18 expects developments to address housing demand, contribute to the diversity of housing in the locality, and respond to the requirements of a changing population. The submitted report prepared by Carter Jonas in support of this application suggests there is a demand for PBSA, as only 35.5% of full time students can access purpose-built university or private sector accommodation, with the remaining 64.5% full time students having to either obtain accommodation elsewhere, often in HMOs or parental homes. Some comments have suggested that the Council has already approved a large amount of student accommodation, the submitted report addresses this, suggesting that if all permitted student accommodation is built out in Bristol, a total of 53.7% of full time students would not have access to university or private sector PBSA, and hence would have to rely on HMOs and parental accommodation. The development would therefore help address a demand for specialist student accommodation, as expected by policy BCS18.

The report also highlights that the number of students in higher education facilities in Bristol is likely to continue to increase in future, and hence the accommodation proposed would help to respond to the requirements of a changing population, in accordance with policy BCS18. Further, as has been discussed, there is limited PBSA in the locality or Ward, which does suggest the proposal would contribute to the mix and diversity of housing types in the area. Finally, it is important to acknowledge the role that PBSA plays in housing Bristol's student population, including reducing pressure on existing housing stock from, for example, HMO conversions.

The development would not create or contribute to a harmful concentration of student uses within the locality, and therefore meets criteria ii of policy DM2. Finally, in accordance with policy BCS18, the

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proposal would help to address a need for student housing in Bristol, which weighs favourably in the planning balance.

iv. The location of Purpose-Built Student Accommodation

The development has been criticised on the grounds of the proposal being located outside of the city centre, in a location perceived by some to be far away from higher education establishments. As has been discussed, there is limited policy-basis to resist the application on these grounds. As this report will demonstrate, the development meets the general criteria of policy DM2, meaning the proposal's location is suitable.

It is also necessary to consider the regeneration goals for South Bristol, specifically policy BCS1 encourages the comprehensive redevelopment of South Bristol, expecting development to occur on previously developed land. Further, policy BCS20 expects new development to re-use previously developed sites, achieving higher densities near centres, and/or close to main public transport routes. The development meets these expectations, as the site represents previously developed land and is within walking distance to main public transport routes, including bus stops on Chessel Street and West Street with regular services to locations where students would want or need to visit.

Chessel Street has a regular bus service to the city centre, which aside from providing access to the centre's amenities and higher education establishments, would also allow access to the University of Bristol's bus services (U1). Further, Chessel Street has a bus service to Frenchay, providing access to the University of the West of England's main campus. There is also a Metrobus service to UWE's Frenchay Campus which is within a 10 minute walk from the site. Also within walking distance to the site is Parson Street railway station, which has a regular service to Bristol Temple Meads, adjacent to the University of Bristol's planned Temple Quarter Campus. A range of amenities are also available at the nearby centres of North Street and East Street, both of which are approximately a 600-700 metres walk from the site. Further, West Street itself has a range of shops, pubs, restaurants and takeaways, all of which could serve and benefit from the development's future residents.

To further illustrate the site's accessibility credentials, the Applicant's Planning Agent has provided the following table, which includes estimated travel times to higher education facilities based on Google Maps:

University / College	Walking	Cycling	Public Transport
UOB Temple Quarter	1.7 miles (35 mins)	1.9 miles (11 mins)	Bus (76) - 21 mins
UWE Bower Ashton	1.5 miles (32 mins)	1.7 miles (9 mins)	
UOB Woodland Road	2.3 miles (50 mins)	2.5 miles (18 mins)	Bus (76) - 24 mins
BIMM	2.1 miles (44 mins)	2.5 miles (13 mins)	Bus (76) - 16 min
University of Law	1.6 miles (32 mins)	1.8 miles (9mins)	Bus (76) – 13 mins
City of Bristol College	1.7 miles (36 mins)	2.1 miles (11 mins)	Bus (76) – 17 mins

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Boomsatsuma	1.6 miles (32 mins)	1.6 miles (9 mins)	Bus (76) - 12 mins
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Members of the public have suggested that the area would benefit from typical residential accommodation (Use Class C3). The Local Plan Review did earmark the site for residential uses. The Local Plan Review also included a policy focussing on specialist student accommodation (draft policy H7), which amongst other things tries to direct student accommodation to defined areas like the city centre or central Bedminster, but importantly the draft policy does allow for the student accommodation in other areas subject to an assessment against criteria. Nevertheless, limited weight can be attributed to this, as the adoption of the Local Plan Review is expected in Autumn 2024. The Council has consulted on a draft SPD for PBSA and shared living, however initial consultation raised a number of significant issues, and hence the draft SPD attracts limited weight in planning assessments at this time. Accordingly, there is no adopted policy expectation for this site to be developed for C3 residential accommodation, and no policy expectation that in principle prevents the site's use as student accommodation.

v. Employment Use

The car showroom is not an employment use as defined by policy DM12, unlike the former car repair shop (Use Class B2). Officers acknowledge the quality of the car repair shop, but still consider that policy DM12 applies. While the proposed replacement employment use will provide less floorspace than the current B2 unit, the replacement E(g) unit will be of higher quality, and hence officers find the proposal to be consistent with policy DM12 iii.

Use Class E(g) represents uses that can be carried out in a residential area without detriment to its amenity, including:

- E(g)(i) Offices to carry out any operational or administrative functions,
- E(g)(ii) Research and development of products or processes
- E(g)(iii) Industrial processes

While these uses must be appropriate for residential areas, there is nevertheless a risk that uses including research and development and industrial process could negatively impact future residents of the proposal, as well as existing neighbours. As such, a range of conditions will be applied, as is discussed within Key Issue C.

vi. Housing Supply

Student accommodation contributes towards the Council's housing supply. Material to the determination of this application is the fact that the Council does not have a 5 Year Housing Land Supply and has failed its most recent Housing Delivery Tests, meaning the tilted test under paragraph 11 d) ii. of the NPPF applies. Given the proposal meets the relevant policies that are considered 'out-of-date', the repercussions of the Council's housing supply has limited impact on the determination of the application, other than to reiterate the presumption in favour of granting planning permission, unless any adverse impacts of doing so would significantly and demonstrable outweigh the benefits. With regard to considering its benefits, to which substantial weight should be afforded, as the proposal would develop a brownfield site for student homes (paragraph 120c of the NPPF).

vii. Summary

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The proposed development is acceptable in principle. Members are reminded that the location of the development is not a reason to refuse the application, as in this circumstance, there is no sound policy-basis to do so. Rather, substantial weight must be attributed to proposal, which will deliver needed student accommodation that also counts towards the Council's five year housing land supply.

(B) Urban Design and Residential Amenity for Future Occupiers

i. Relevant Policy, Guidance and Material Considerations

Relevant planning urban design policies include: BCS18 'Mix and Balance', BCS20 'Effective and Efficient Use of Land', BCS21 'Quality Urban Design', BCS22 'Conservation and the Historic Environment', BCS23 'Noise / Pollution', DM2 'Specialist Accommodation', DM26 'Local Character and Distinctiveness', DM27 'Layout and Form', DM28 'Public Realm', DM29 'Design of New Buildings', DM35 'Noise / Pollution', and the UL SPD.

ii. UL SPD

The UL SPD includes questions regarding development quality that are designed for applicants, local authority planners and other stakeholders to use throughout the design development of a scheme. Accordingly, the UL SPD represents guidance as to how to assess developments against Development Plan policies. Officers will therefore consider the guidance (and questions) included within the UL SPD within this section as an aid to assess the quality of the development regarding relevant design policies.

iii. UL SPD Assessment – Part 1: Guidance for all major developments - City

Question 1.1 asks if the scheme adopts an approach to urban intensification which is broadly consistent with its setting. The proposal's design approach is successful in repairing the street scene and restoring coherent built form that appropriately addresses the existing buildings on West Street, including the block of flats in Chapel Barton. This is evident in the development's height, scale, and massing, which is consistent with the buildings in West Street. The rear element of Block A has a lower height than the Chapel Barton flats, which justifies its scale and height, particularly with regard to the expectations of policy DM26's guidance for backland development.

A significant number of bedspaces are proposed by the development, but it is considered to represent an appropriate density for this site, as is evident in its appearance. The remaining report assesses the other aspects of the development, some of which are relevant to the density of the proposal, such as the residential amenity of future residents.

iv. UL SPD Assessment – Part 1: Guidance for all major developments - Neighbourhood

Question 1.2 considers a development's impacts on the neighbourhood and asks if a development would contribute positively towards creating a vibrant and equitable neighbourhood. The application was supported by a statement of community involvement that has already been discussed. Further, it has been established within Key Issue A that the site is in a sustainable location with good access to public transport and amenities. Further, Key Issue A discusses the proposal's potential to help to contribute to vibrant, mixed and balanced neighbourhoods. The proposals are also adaptable, some

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would lend themselves to conversions in future to other residential uses (subject to planning permission). As is encouraged by the UL SPD, the proposal vertically stacked uses, including an employment use on the ground floor of Block A.

Question 1.3 asks if the scheme responds positively to either the existing context, or in areas undergoing significant change, an emerging context. This has been discussed under Q 1.1 of the UL SPD, nevertheless the proposal respects the existing context through its appropriate scale and mass, and use of materials and detailing that are consistent with the prevailing character of the area.

v. UL SPD Assessment – Part 1: Guidance for all major developments - Block and Street

Question 1.4 concerns how buildings relate to streets, asking if a scheme would provide people-friendly streets and spaces. The development responds well to the existing street layout, and would increase the passive surveillance of West Street. The ground floor employment unit and lobby will help provide a suitable active street scene, and generally the front elevation includes sufficient visual interest to engage well with the street scene.

In accordance with Question 1.5, the proposal would deliver a comfortable micro-climate for its occupants, neighbours and passers-by. Key Issue D is relevant to Question 1.6, which concerns access, car parking and servicing, including cycle parking.

vi. UL SPD Assessment – Part 2: Guidance for all major developments - Shared Access and Internal Spaces

Question 2.1 concerns how attractive and welcoming development's accesses. Question 2.2 asks whether schemes will provide internal spaces that are convivial, comfortable and user-friendly.

The proposal includes a lobby area on the ground floor that would welcome residents to Block B, and also provide space for residents of Block A (as well as Block B) to speak with the management of the development, collect post, and meet with other residents. Block A's main access for residents is from Chapel Barton. The access is well-lit by natural light and directly leads to the rear garden, whilst also providing access to the main core of the building. The accesses are understood to be wheelchair accessible and user friendly. Each cluster flat includes adequate communal space for residents, and the TV room within the lobby will provide a communal area for residents of the studio flats to meet with others, albeit the studio flats are sufficiently sized to accommodate visitors. Overall, the development meets the expectations of the UL SPD with regard to questions 2.1 and 2.2.

vii. UL SPD Assessment – Part 2: Guidance for all major developments - Outdoor Spaces

Questions 2.3 and 2.4 concern outdoor space, and whether it's sufficient in size and quality.

The UL SPD advises that proposals should provide 5sq.m of private outdoor space for a 1-2 person unit and an extra 1sq.m for each additional occupant. The outdoor space should be provided as private balconies or gardens, or as communal gardens and roof terraces. Applying this guidance to the development, the proposal should provide a minimum 118sq.m of external amenity space, which the proposal exceeds through its two garden areas that cumulatively provides approximately 160 sq.m of gross external amenity space and 120sq.m of net external amenity space (i.e. taking into account purely landscaped spaces). Comprehensive landscape plans have been provided for the external spaces, which are considered to represent attractive, well designed and easily managed outdoor

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spaces. The development therefore meets the UL SPD's expectations for outdoor spaces. Officers realise some other PBSA proposals in the city have benefited from greater levels of external amenity space, but as the proposal meets the UL SPD's guidance, and there are other opportunities for outdoor amenity space within walking distance of the site (e.g. South Street Park), officers are confident that residents will benefit from sufficient levels external amenity space.

Question 2.5 concerns children's play and is therefore not relevant to this development.

viii. UL SPD Assessment – Part 2: Guidance for all major developments - Individual Homes

Policy BCS18 expects residential development to provide sufficient space for everyday activities and enable flexibility and adaptability by meeting appropriate space standards. Policy BCS21 expects development to create a high quality environment for future occupiers while safeguarding existing surrounding development. Policy DM2 also requires developments to provide acceptable living environments. The Urban Living SPD sets out requirements for achieving good quality residential developments at higher densities. The questions associated with 'Individual Homes' in the UL SPD concerns:

- Question 2.6 - Whether the proposal's internal layouts are ergonomic and adaptable.
- Question 2.7 – Does the scheme safeguard privacy and minimise noise transfer between homes?
- Question 2.8 – Does the scheme maximise opportunities for natural illumination of internal spaces avoiding single aspect homes?

Although policy DM4 is not applicable to this development, the proposal includes 2no. wheelchair accessible units (M4(3)), these units are Flats 1 and 2 in Block A. Question 2.6 of the UL SPD recommends that 90 per cent of new build housing meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings' with the remaining 10 per cent meeting Building Regulation M4(3) 'wheelchair user dwellings'. 10.5% of the homes will be M4(3) compliant, meeting the UL SPD's expectations, the applicant has not confirmed the number of M4(2) compliant flats. The proposal would exceed the principle policy requirement (Policy DM4), and the UL SPD's expectations for M4(3) flats, but falls short of the guidance issued within the UL SPD in relation to M4(2) provision. Whilst this is disappointing with regard to M4(2) units, the development meets policy and guidance expectations with regard to M4(3) compatibility, which is arguably more valuable and in demand. Further, Members will be aware of the emerging policies in this regard, which will address this discrepancy between planning policy and guidance.

A single car parking space for disabled people is provided, which meets the policy expectations set out in Appendix 2 of the Site Allocations and Development Management Plan. Officers raise no objection to accessibility and this development and consider that the local planning authority has suitably discharged its duty under the Equalities Act 2010 to not discriminate when exercising its public function.

The Council does not have space standards for student accommodation, but the proposal compares favourably against previously approved PBSA in Bristol. Further, the proposed flats would likely meet the HMO SPD's space standards, which although are not applicable to the development, do provide helpful guide. In accordance with the Urban Design Team's comments, the proposal has been amended so the cluster flats include no more 6 rooms per flat. Rooms are proposed for single

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occupancy, including studio flats, a condition is recommended to ensure that flats are let as such, if flats/rooms were occupied by more than one resident, the sizes of the flats/rooms would be insufficient.

The majority of the flats are dual aspect (10), and of the single aspect second floor flats, all of have rooflights as well as windows to maximise levels of natural light. Most rooms would benefit from acceptable levels of outlook. Officers acknowledge that a small minority of the flats have angled or oriel windows to prevent overlooking, but in each case acceptable standards of outlook would be gained from the respective flats. Each flat would benefit from appropriate levels of privacy. The impact of the development on neighbour's amenity is addressed in Key Issue C. Triple glazed windows are proposed, and the Pollution Control Team has raised no objection with regard to noise from the road detrimentally impacting the amenity of future residents. The development is considered to provide acceptable living environments for future residents.

The Crime Reduction Advisor offers no objections to the development, but has raised some limited concerns in relation to building standards, most of which will be addressed by Building Regulations. In accordance with the Crime Reduction Advisor's comments, it is recommended that a scheme of external lighting is secured by condition.

Overall, the proposal would provide an acceptable standard of internal amenity for future residents of the development hereby approved.

ix. General Appearance Assessment

The scale, height and massing of the development have been assessed and found acceptable. The principal elevation of the development takes inspiration from existing characterful buildings in West Street. The predominant material is red brick, with a stone coping detail framing windows and doors. A pleasing chamfered corner approach is taken on the corners of Blocks A and B where they meet Chapel Barton. The proposal's scale between 2.5 to 3 storeys is suitably treated with the mansard roof behind a parapet wall detail. The rear of Block A has two distinct phases, the first follows the same detailing approach to the principal elevation, and the second introduces different materials (standing seam metal roof, different window framings, and distinct brick features). The rear elevations of the buildings largely follow the same approach as rear section of Block A, which is appropriate. The rear elevation of Block B also incorporates green infrastructure to soften the appearance. Further, the side elevation of the proposal facing Churchlands Road is proposed to incorporate artwork, which is recommended to be secured by condition. Overall, the proposal's general appearance is appropriate and will enhance the character of the area.

x. Landscape and Green Infrastructure

In accordance with policies BCS9 and DM29, the proposal delivers green infrastructure of an appropriate type, standard and size. The existing site has very limited green infrastructure, albeit there are a small number of trees adjacent to the site that will not be harmed by the proposal. The proposal incorporates 6 new trees to be planted within the rear gardens. Further, the rear elevation of Block B, which is visible from Churchlands Road, will incorporate planted climbers that will be supported by a Jakob Green Wall system.

xi. Impact on Future Development

Development should not prejudice the future development of adjacent sites. In this case, it is reasonable to expect the Christian Centre to be redeveloped at some point in the future, albeit officers

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are not aware of any current plans. Specifically, the Churchlands Road side elevation does not include any side-facing windows meaning it could be extended in future toward the corner of the site and to the rear, in a similar way to existing terraced streets that meet West Street. Officers do realise that this would require the removal of the public art, but this is not an insurmountable hurdle for future development. Further, the future redevelopment of the Christian Centre along the line nos. 11-21 Churchlands Roads would be possible in principle, subject to suitable distancing between the proposed rear elevation and the potential future development.

xii. Summary

The development represents an improvement to the visual appearance of the site when compared to the existing buildings at the site. Further, the proposal will deliver a high-quality living environment for future residents. The development largely complies with the Urban Living SPD's guidance and all relevant urban design-related planning policies. Overall, officers advise that the proposal's quality of urban design should attract positive weight in favour of approving the planning application.

(C) Impact on Neighbouring Properties

In relation to policy DM2 and neighbour's residential amenity, the key question is whether levels of activity related to the development will cause or contribute to excessive noise and disturbance to residents. Other policies also require an assessment of the proposal's impact on the levels of neighbour's privacy, outlook and light.

i. Noise and Disturbance

The Pollution Control Team has raised no objection to the development in relation to noise or disturbance, and according to the planning register, there are no existing nearby student residents, hence the proposal is unlikely to contribute to a harmful concentration of student uses. It is important to not discriminate against the future residents of the development based on assumptions concerning age or the nature of students. Each flat has access to internal communal amenity areas that will help focus activities within the development, rather than externally. Whilst garden areas are proposed, the landscape design will limit the potential for large congregations of residents that could potentially result in excessive levels of noise. Importantly, the application is supported by a management plan, which includes measures that will help limit the potential for disturbance to neighbours through noise and other activities, these are listed below and are advised to be secured by condition:

- An on-site accommodation manager who will be on-site 09:00-17:00 (Monday to Friday), with 24/7 supervision provided by support staff;
- CCTV and audio detection with alarms for support staff if excessive levels of noise are detected;
- Site access 24/7 for serious issues;
- Visible staff presence through the management office near the entrance;

The proposal also include air source heat pumps that do generate noise, and as such a condition is recommended to ensure an acceptable impact on existing and future residents.

ii. Overlooking / Privacy

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The proposed development is carefully designed to avoid harmful impacts to neighbour's privacy, outlook or levels of light. The proposal would not result in any harmful overlooking to the homes to north east in Bartlett's Road, as any direct window-to-window distances are in excess of 21 metres. Further, the windows within the north-eastern elevation of Block A would not harmfully overlook the rear gardens in Bartlett Road, due to the intervening distances. The very rear of no. 1 Bartlett Road's rear garden would experience overlooking as a result of the development, but not in an unacceptable manner, largely as any views of the most sensitive part of no. 1 Bartlett's Road's rear garden adjacent to its rear elevation would mostly be blocked by the existing garage. It also material that this overlooking is seen elsewhere in the locality, namely from the rear elevation of the block of flats in Chapel Barton. The proposal would not result in harmful overlooking to the rear of no. 153 West Street, as proposed windows are sensitively located to avoid direct views into the garden area.

The rear windows of Block B would look toward the single storey Christian Centre building, which has windows facing the development. Unacceptable overlooking will not occur, as the Christian Centre's windows are obscure and are understood to only provide light to a corridor.

iii. Levels of Light / Overshadowing

The rear elevations and gardens of the homes in Bartlett's Road currently experience overshadowing from the Chapel Barton flats. According to the submitted overshadowing assessment, the proposal would not materially increase the amount overshadowing these properties would experience. The development is to the north of the Christian Centre meaning overshadowing will not occur.

iv. Overbearing and Outlook

The Christian Centre does have a small patch of garden immediately to the rear of Block B, but the amenity value of this garden space to the Christian Centre does not render the proposal's impact unacceptable. Further, the mass and height of the proposal would not harm the outlook of the Centre, or the result in an overbearing impact. Due to the distance between Block A and the rear elevations of the houses in Bartlett's Road, the proposal would not have an overbearing impact, and nor would it harm the neighbours outlook.

v. Management of the Employment Use

As per the Pollution Control Team's comments, there are some concerns that the employment floorspace, which could be used for appropriate industrial processes, could materially harm the amenity of existing and future residents. It would be inappropriate for any planning permission to restrict the use of industrial or research uses allowed under Use Class E(g), as the only permitted uses are those that are appropriate for residential areas. Further, the employment unit is of a modest size, meaning the scope of disturbance from, for example large industrial uses, is low. Conditions however are needed to manage the employment floorspace to reduce the potential for disturbance to neighbours, and as such are advised.

vi. Summary

The proposal is not expected to unacceptably harm the residential amenity of neighbours.

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(D) Highway Safety and Transportation

Policy DM2 resists development that would harm the character of the locality as a result of levels of on-street parking that cannot be reasonably accommodated or regulated through parking control measures. The policy also resists proposals where inadequate storage for recycling/refuse and cycles is provided. Policy DM23 resists developments that would give rise to unacceptable traffic conditions.

In support of the application, a Transport Statement and Travel Plan were initially provided, and in response to Transport Development Management's concerns, a Technical Note was provided by Transport Planning Associates.

Transport Development Management (TDM) support the principle of development, and following the submission of the Technical Note and revised plans raise no objection to the application.

i. Car Parking

Neighbours have raised concern relating to a perceived under provision of on-site car parking for future residents of the development, and an existing under provision of on-street car parking within the locality that will be exacerbated by this development. The proposal includes two car parking spaces, one of which is a wheelchair accessible space. A car club space will also be provided in the locality.

In relation to car parking, policy DM23 expects developments to: "Provide an appropriate level of safe, secure, accessible and usable parking provision having regard to the parking standards, the parking management regime and the level of accessibility by walking, cycling and public transport".

The locality is not subject to a parking management regime, such as an RPZ, meaning future residents of the site would be able to park a car in the vicinity. However, officers do understand that car ownership is generally lower amongst students than the general population, and as Key Issue A established, the site benefits from a good level of accessibility by public transport, and many amenities are within walking distance. The Council does not have minimum car parking standards for most accommodation types, apart from in relation to spaces accessible to disabled people, which this development meets through the provision of a single wheelchair accessible space. Taking this into account, it is only reasonable to refuse planning permission on the basis of insufficient car parking spaces, if it can be demonstrated that car parking associated with the proposal would harm the character of the locality as a result of levels of on-street parking, including highway safety.

TDM initially raised concerns regarding a lack of an acceptable parking survey. Following these concerns, the applicant provided a parking survey, revised car ownership data and provision for a car club in the locality. Although TDM criticised the method used to calculate the likely car ownership resulting from this development within the Technical Note and Transport Statement, TDM advised that the parking survey provided by the applicant suitably evidenced that there is parking availability within the area (at least 21 spaces). Based on census data and experience of student developments in Bristol, TDM expect car ownership associated with this development to be more than 15 cars, albeit TDM stress this an upper estimate. This assessment, together with the provision of a car club space, demonstrated to TDM that the development would not give rise to unacceptable levels of on-street parking in the vicinity. With this in mind, officers advise that the development provides sufficient car parking spaces.

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To secure the car club space, the applicant will be required to pay a contribution of £6,310 towards a Traffic Regulation Order associated with the car club space e.g. lining and signage. This will be secured by s.106 Agreement. Furthermore, the applicant will be required to procure the car club operator and as should be set out within the travel plan statement provide car club memberships to residents of the development.

ii. Cycle Storage

19 cycle space are proposed within the ground floor bike store, accessed from Chapel Barton. As per TDM's comments, this provision exceeds the minimum standards set out in Appendix 2 to the SADMP.

iii. Servicing and Waste

TDM has confirmed that the refuse and recycling store located at the front of the building on the ground floor is sufficient in size to meet the needs of the development.

iv. Highway works

Should a permission be granted, highway works are required to be implemented such as the resurfacing of the footway, upgrade (or relocation) to the street lighting given the difference in scales between the existing and proposed buildings and the refresh of street lining where necessary. A condition is recommended in this respect.

v. Construction Management

A construction management plan condition is advised to ensure an acceptable highway safety and transport impact during construction.

vi. Summary.

In summary, the development is not expected to have a harmful impact on the amenity of the highway in the area, including the development's highway safety impact. Officers understand neighbours' concerns relating to the proposal's car parking provision, but as discussed, planning policy does not expect proposal's to provide a minimum number of car parking spaces. Further, the development includes a car club space in the locality that will help reduce reliance on personal car ownership. Finally, as TDM has confirmed, there is capacity in the locality to address car ownership/use related to this development.

(E) Sustainability

The proposal is supported by an Energy and Sustainability Statement to address how the proposal will comply with relevant planning policies.

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The proposal includes air source heat pumps for space and water heating within the development, which is compliant with policy BCS14's energy hierarchy. The proposal also incorporates photovoltaic panels, which together with the air source heat pump system equates to a 38.4% reduction in emissions through renewables, which exceeds policy BCS14's expectations.

In accordance with policy BCS13, the proposal is supported by an overheating assessment, which demonstrates that with mitigation measures (natural ventilation), the proposal meets the relevant guidance.

Further sustainability measures are proposed, including building construction using the Green Life Buildings (GLB) SIP system, which achieves extremely low U Values, low thermal mass and improved psi values and air tightness. Windows will be triple glazed with low U Values and MVHR will be installed as well as 100% low energy lighting.

In accordance with the Sustainability Team's comments who support the application, conditions are recommended.

(F) Other Matters

- i. **Air Quality** – the Air Quality Team raises no objection to the development.
- ii. **Land Contamination** - the Council's Land Contamination Officer raises no objection to the development, but has recommended a number of standard conditions to address any contamination and unexploded ordnance at the site that may be present. Such conditions are recommended to be applied if the application is approved.
- iii. **Arboriculture** – outside of the site adjacent to Churchlands Road is patch of land understood to form part of the Christian Centre. The land hosts a number of trees that could be impacted by the development. To assess the impact, an arboricultural method statement was submitted by the applicant. The Council's Arboricultural Officer assessed the statement, concluding it includes sufficient measures to appropriately protect the trees. Appropriate conditions are advised to secure the tree protection measures.
- iv. **Nature Conservation** - the application is supported by a preliminary ecological assessment that considers the existing site to have a low ecological value, including limited habitats. The Nature Conservation Officer raises no issue with the submitted ecological assessment and generally no objection to the development subject to conditions. The development is excepted to deliver a net gain for biodiversity in accordance with the NPPF. This is because the existing site does include limited habitats and the proposal include sufficient landscaping. A condition is recommended to secure this. Further, it is recommended that further ecological enhancement measures are secured via condition, including facilities (nests/roosts) for bats, birds, insects, and hedgehogs. Finally, the Nature Conservation Officer recommend a condition to secure living roofs, however officers do not find such a condition to be necessary to meet relevant policies, given the proposal's inclusion of sufficient green infrastructure elsewhere in the proposal.
- v. **Site Drainage** - concerns have been expressed by the Lead Local Flood Authority (LLFA) in relation to the drainage proposals, largely as it is a pump-based system and not a gravity-based

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system. The LLFA agree their concerns can be addressed by a condition requiring a revised scheme of drainage, and hence such a condition is recommended.

- vi. **Equalities Impact Assessment** - during the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. Overall, it is considered that the approval of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

(G) Planning Obligations

Planning obligations must accord with section 122 of the Community Infrastructure Levy Regulations 2010 and paragraphs 55 and 57 of the NPPF, in that a planning obligation may only constitute a reason for granting planning permission for a development if the obligation is:

- a) necessary to make the development acceptable in planning terms;
- b) directly related to the development; and
- c) fairly and reasonably related in scale and kind to the development.

Further, policy BCS11 expects developments to provide or contribute towards the provision of measures to directly mitigate its impact, either geographically or functionally, which will be secured through the use of planning obligations.

Officers advise that the recommended planning obligations discussed in Key Issue D meets the requirements set out above. As such, officers recommend that should the application be approved, a s106 Agreement (Unilateral Undertaking) is drafted and agreed with the applicant to secure the following planning obligation:

- TRO Contribution - the contribution in the sum of six thousand three hundred and ten pounds (£6,310) Indexed Linked payable to the Council by the Owner prior to the commencement of development. This is to secure a car club space within the highway.

The applicant has agreed in principle to this contribution.

Community Infrastructure Levy (CIL) – the development will be liable to pay £138,761.27.

(H) Planning Balance

As the Council does not have a 5 Year Housing Land Supply and has failed its most recent Housing Delivery Tests, there is a presumption in favour of granting planning permission, unless any adverse impacts of doing so would significantly and demonstrable outweigh the benefits. This report demonstrates there are no significant or demonstrable harms associated with approving this development. The proposal complies with policy DM2, which sets out the general criteria that PBSA must meet. Importantly, there are significant benefits that would flow from approving this application.

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Perhaps most significantly, the proposal would deliver the redevelopment of brownfield land to deliver needed student accommodation, that would help meet demand and diversify the housing mix, as well as contributing to the Council's 5 Year Housing Land Supply. When compared to the existing building quality at the site, the proposal would enhance the character of the area, which must attract positive weight in favour of approving the planning application. Further, the proposal incorporates photovoltaic panels, which together with the air source heat pump system equates to a 38.4% reduction in emissions through renewables, which exceeds policy BCS14's expectations. The development is expected to deliver a biodiversity net gain. The proposal would also deliver improved employment floorspace at the site.

The application is therefore recommended for approval subject to conditions and a s.106 Agreement, in accordance with Key Issue I.

(I) Recommendation

It is recommended that the application is granted planning permission, subject to a Planning Agreement and conditions, as per the instructions listed below:

- A. That the Applicant be advised that the Local Planning Authority is disposed to grant planning permission, subject to the completion, within a period of six months from the date of this committee, or any other time as may be reasonably agreed with the Service Director, Planning and Sustainable Development and at the Applicant's expense, of a planning agreement made under the terms of Section 106 of the Town and Country Planning Act 1990 (as amended), entered into by the Applicant to cover the following matter:

TRO Contribution - the contribution in the sum of six thousand three hundred and ten pounds (£6,310) Indexed Linked payable to the Council by the Owner prior to the commencement of development. This is for the car club space that will be located in the local vicinity to the application site.

- B. That the Head of Legal Services be authorised to conclude the Planning Agreement to cover matters in recommendation (A).
- C. That on completion of the Section 106 Agreement, planning permission be granted, subject to conditions listed below:

Time limit for commencement of development

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

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2. Construction Management Plan

No development shall take place, including any demolition works, until a construction management plan or construction method statement has been submitted to and approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the demolition/construction period. The plan/statement shall provide for:

- 24 hour emergency contact number;
- Hours of operation;
- Parking of vehicle of site operatives and visitors (including measures taken to ensure satisfactory access and movement for existing occupiers of neighbouring properties during construction);
- Routes for construction traffic;
- Locations for loading/unloading and storage of plant, waste and construction materials;
- Method of preventing mud being carried onto the highway;
- Measures to protect vulnerable road users (cyclists and pedestrians)
- Any necessary temporary traffic management measures;
- Arrangements for turning vehicles;
- Arrangements to receive abnormal loads or unusually large vehicles;
- Methods of communicating the Construction Management Plan to staff, visitors and neighbouring residents and businesses.

Reason: In the interests of safe operation of the adopted highway in the lead into development both during the demolition and construction phase of the development.

3. Highway Condition Survey

No development shall take place (including investigation work, demolition, siting of site compound/welfare facilities) until a survey of the condition of the adopted highway has been submitted to and approved in writing by the Local Planning Authority. The extent of the area to be surveyed must be agreed by the Highways Authority prior to the survey being undertaken. The survey must consist of:

- A plan to a scale of 1:1000 showing the location of all defects identified;
- A written and photographic record of all defects with corresponding location references accompanied by a description of the extent of the assessed area and a record of the date, time and weather conditions at the time of the survey.

No building or use hereby permitted shall be occupied or the use commenced until any damage to the adopted highway has been made good to the satisfaction of the Highway Authority.

Reason: To ensure that any damage to the adopted highway sustained throughout the development process can be identified and subsequently remedied at the expense of the developer.

4. Protection of Retained Trees during the Construction Period

No work of any kind shall take place on the site until the protective fences have been erected around the retained trees in the position and to the specification shown on the Bosky Trees Tree Constraints Plan Dwg TCP-1. The approved fence(s) shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of the development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Within the fenced area(s) there shall be no scaffolding, no

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stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the council.

Under no circumstances should the tree protection be moved during the period of the development and until all works are completed and all materials and machinery are removed. Landscaping works within protected areas is to be agreed with the Local Planning Authority and carried out when all other construction and landscaping works are complete.

Reason: To protect the retained trees from damage during construction, including all ground works and works that may be required by other conditions, and in recognition of the contribution which the retained tree(s) give(s) and will continue to give to the amenity of the area in line with Policy DM17.

5. Arboricultural method statement - During construction

The applicant/developer shall ensure that all works within the root protection area of retained trees, must follow the detailed methodology with the approved Arboricultural Method Statement (Bosky trees). In the instance that major roots are found then further consultation with an arboriculturist will be required, any changes to the specified methodology must be agreed in writing by the local planning authority.

Reason: To protect the retained tree from damage during construction and in recognition of the contribution which the retained tree gives and will continue to give to the amenity of the area.

6. Arboricultural Supervision

No development shall take place until a pre-commencement site meeting has been held and attended by the developer's project arboricultural consultant and the designated site manager to discuss details of the working procedures set out in the approved Arboricultural Method Statement. A schedule of visits shall be agreed to ensure the project arboriculturist is present during key stages of the development which include, but not limited to the installation of scaffolding and low level scaffolding ground protection deck. Site visits by the project arboriculturist must be carried out during the key stages identified above.

In the event remedial works are considered necessary, copies of the required remedial works shall be submitted to and approved in writing by the Local Planning Authority, prior to the first occupation of the development hereby approved. Any approved remedial works shall subsequently be carried out under strict supervision by the arboricultural consultant immediately following that approval.

Reason: In order that the Local Planning Authority may be satisfied that the trees to be retained on-site will not be damaged during the construction works and to ensure that as far as possible the work is carried out in accordance with current best practice

7. Intrusive site investigation

No development shall commence on site (except demolition) until a report detailing the results from a site specific risk assessment and intrusive investigation has been submitted to and approved in writing by the Local Planning Authority. The investigations shall assess the nature and extent of any site contamination and whether or not it originates from the site. The investigation and risk assessment must be undertaken by competent persons and a written

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report of the findings must be produced. The results of this investigation shall be considered along with the reports submitted with the original application. This must be conducted in accordance with the Environment Agency's 'Land Contamination: risk management' and BS 10175:2011 + A2:2017: Investigation of Potentially Contaminated Sites - Code of Practice.

Reason: To ensure that risks from land contamination is understood prior to works on site both during the construction phase to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

8. Submission of Remediation Scheme

No development shall take place (except demolition) until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment has been prepared, submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination is understood prior to works on site both during the construction phase to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

9. Implementation of Approved Remediation Scheme

In the event that contamination is found, no occupation of the development shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (otherwise known as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination is understood prior to works on site both during the construction phase to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

10. Unexploded Ordnance

No development shall take place until an unexploded ordnance survey, along with the results, has been submitted to and approved in writing by the Local Planning Authority. The survey shall establish whether there is any unexploded ordnance, the details of which shall include any necessary mitigation measures and shall be submitted to the local planning authority for approval. The development shall be undertaken in full accordance with any approved mitigation measures.

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Reason: To ensure that development can take place without unacceptable risk to workers and neighbours including any unacceptable major disruption to the wider public on and off site that may arise as a result of evacuation/s associated with the mitigation of UXO.

11. Construction Environmental Management Plan

No development shall take place until a site specific Construction Environmental Management Plan has been submitted to and been approved in writing by the Council. The plan must demonstrate the adoption and use of the best practicable means to reduce the effects of noise, vibration, dust and site lighting. The approved plan/statement shall be adhered to throughout the demolition/construction period.

Reason: In the interests of the amenities of surrounding occupiers during the construction of the development.

Advice: See Informative Notes/Advice Notes for guidance.

12. Public Art

Prior to the commencement of above ground construction, a Public Art Plan indicating the public art to be installed on the north east elevation (Churchlands Road) shall be submitted to and approved in writing by the Local Planning Authority. In addition to details of the public art installation(s), the Public Art Plan shall contain a timetable for delivery, details of the Public Art commission, and details of future maintenance responsibilities and requirements. All public art works shall be completed in accordance with the agreed scheme and thereafter retained as part of the development, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that public art is integrated into the design and build of the development.

13. Design of New Building

Detailed drawings including plans, sections and elevations at a relevant scale between 1:5 and 1:20 of the following shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The works shall be carried out in accordance with the approved details.

- a. Typical details of all types of doors and window opening including sills, lintels, surrounds, reveals and design of external doors and windows.
- b. Typical details of the roof, including key junctions with the elevation and the parapet wall
- c. Typical details of the office frontages.
- d. Typical details of eaves, soffits, overhangs, ridge, parapets, stone coping.
- e. Typical details of rainwater goods.

Reason: In the interests of visual amenity, the character of the area, and the amenity of residents.

14. Materials

A materials schedule for all external facing elements of the development hereby approved, shall be submitted to and be approved in writing by the Local Planning Authority before the relevant part of work is begun. The schedule shall include specification, location, and a visual impression of all external facing materials to be used in the development hereby approved. The works shall be carried out in accordance with the approved details.

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Reason: In the interests of the appearance of the building itself and the wider appearance of the area.

15. Renewable energy - Air source heat pumps

Prior to the relevant stage of the development hereby approved, details of the air source heat pumps (including the exact location and design/ technical specification) together with calculation of energy generation and associated CO2 emissions to achieve a reduction on residual emissions from renewable energy in line with the approved energy statement should be submitted to the Local Planning Authority and approved in writing. The system shall be installed prior to occupation of the dwellings and thereafter retained.

Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions.

16. Photovoltaic Panels

Prior to the relevant stage of the development hereby approved, details of the proposed PV system including location, dimensions, design/ technical specification together with calculation of annual energy generation (kWh/annum) and associated reduction in residual CO2 emissions shall be provided within the Energy Statement. Prior to occupation the following information shall be provided:

- o Evidence of the PV system as installed including exact location, technical specification and projected annual energy yield (kWh/year) e.g. a copy of the MCS installer's certificate.
- o A calculation showing that the projected annual yield of the installed system is sufficient to reduce residual CO2 emissions by the percentage shown in the approved Energy Statement.

Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions

17. Employment Floorspace - Class E(g)(ii) & (iii) Uses Only

Prior to the commencement of any uses defined as Class E(g)(ii) and (iii) within office floorspace (as labelled on the approved plans) hereby approved, an assessment for the potential for noise from those uses affecting residential properties in the area shall be submitted to and approved in writing by the Local Planning Authority.

If the assessment indicates that noise from the development is likely to affect neighbouring residential or commercial properties, a detailed scheme of noise mitigation measures shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the use.

The approved scheme of noise mitigation shall then be installed prior to the first use of the Class E(g)(ii) and (iii) floorspace and maintained thereafter for the lifetime of the development.

The noise assessment shall be carried out by a suitably qualified acoustic consultant/engineer and shall take into account the provisions of BS 4142:2014+A1:2019 Methods for rating and assessing industrial and commercial sound and BS 8233: 2014 Guidance on sound insulation and noise reduction for Buildings.

Reason: In the interests of the amenity of neighbours and future occupiers of the development.

18. Noise from Plant & Equipment to be installed , including air source heat pumps

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Prior to the installation of any external plant or equipment associated with the development hereby approved, an assessment to show that the rating level of any plant and equipment will be at least 5 dB below the background level shall be submitted to and approved in writing by the Local Planning Authority. The assessment must be carried out by a suitably qualified acoustic consultant/engineer and be in accordance with BS4142: 2014 Methods for rating and assessing industrial and commercial sound. The plant or equipment shall then be installed and maintained thereafter for the lifetime of the development in accordance with the approved details.

Reason: In the interests of the amenity of neighbours and future occupiers of the development.

19. Biodiversity Net Gain

Prior to the relevant stage of the development hereby approved, a Biodiversity Net Gain Assessment shall be submitted to and approved in writing by the Local Planning Authority. The Assessment shall:

Employ the latest version of the Defra / Natural England Biodiversity Metric to demonstrate a biodiversity net gain;

Include full details of the data, measurements and workings used to calculate the percentage BNG must be provided, including habitat condition assessments; and

Include a 30-year Landscape and Ecological Management Plan (LEMP). This should address retained features of ecological interest, together with mitigation and enhancements to be provided, including those included within the approved landscape plan. The LEMP must set out management compartments, objectives, and prescriptions. It must also show how management of the site will be resourced and monitored.

The development shall then be implemented and maintained thereafter for the lifetime of the development in accordance with the approved Biodiversity Net Gain Assessment (and LEMP).

Reason: Ecological enhancement is needed to meet the requirements of the revised National Planning Policy Framework (NPPF, 2021). The NPPF states in paragraph 174 (d) on page 50 that "Planning policies and decisions should contribute to and enhance the natural and local environment by... minimising impacts on and providing net gains for biodiversity...".

20. Ecological Enhancement

An Ecological Mitigation & Enhancement Strategy (EMES) shall be submitted to and approved in writing by the Local Planning Authority before the relevant part of work is begun. This must include details and plans of the provision of integrated (built-in) boxes for birds (2no.), bats (1no.), insects(1no.), and hedgehogs (1no.), as well as the location, specification, height and orientation of these features. The approved enhancement measures shall be implemented prior to the first occupation of the development hereby approved.

Reason: (1) The Natural Environment and Rural Communities (NERC) Act 2006 (Section 40) obliges the LPA '... in exercising its functions, [to] have regard, so far as is consistent with the proper exercise of those functions, to the purpose of conserving biodiversity'. In order to discharge its biodiversity duty, the LPA must satisfy itself that all developments deliver ecological enhancement wherever reasonably possible; (2) Ecological enhancement is a requirement of the revised National Planning Policy Framework (2021) which states (in paragraph 174) that 'Planning policies and decisions should contribute to and enhance the natural and local environment...'.

21. Sustainable Drainage System (SuDS)

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No development shall take place (with the exception of demolition) until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

Pre occupation condition(s)

22. Scheme of External Lighting

Prior to the first occupation of the development hereby approved, a scheme of external lighting shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include the location, type, expected lux levels, and design of the external lights. The scheme of external lighting shall then be implemented in accordance with the approved details prior to the first occupation of the approved development.

Reason: In the interests of amenity of future residents accessing the building safely, and the general amenity of Chapel Barton.

23. Broadband

In accordance with the approved Broadband Connectivity Statement, The homes and workplaces within the development hereby approved will include the provision of high-speed broadband access, that enables the provision of Next Generation broadband, prior to the first occupation of the development hereby approved.

Reasons: In the interests of the amenity of future occupiers, including the enhancing digital inclusion.

24. Energy and Sustainability in accordance with statement

The development hereby approved shall incorporate the energy efficiency measures, renewable energy, sustainable design principles and climate change adaptation measures into the design and construction of the development in full accordance with the sustainability statement and energy strategy (JMDC SERVICES LTD, Rev A, 21 Sept 2022). A total 46% reduction in carbon dioxide emissions beyond Part L 2013 Building Regulations in line with the energy hierarchy shall be achieved, and a 38% reduction in carbon dioxide emissions below residual emissions through renewable technologies shall be achieved.

Reason: To ensure the development incorporates measures to minimise the effects of, and can adapt to a changing climate in accordance with policies BCS13 (Climate Change), BC14 (sustainable energy), BCS15 (Sustainable design and construction), DM29 (Design of new buildings)

25. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

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Reason: In the interests of highway safety.

26. Completion and Maintenance of Car/Vehicle Parking - Shown on Approved Plans

No building or use hereby permitted shall be occupied or use commenced until the car/vehicle parking area (and turning space) shown on the approved plans has been completed and thereafter the area shall be kept free of obstruction and available for the parking of vehicles associated with the development. Driveways/vehicle parking areas accessed from the adopted highway must be properly consolidated and surfaced, (not loose stone, gravel or grasscrete) and subsequently maintained in good working order at all times thereafter for the lifetime of the development.

Reason: To ensure that there are adequate parking facilities to serve the development constructed to an acceptable standard.

27. Completion and Maintenance of Cycle Provision - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

28. Travel Plan Statement - Not Submitted

No building or use hereby permitted shall be occupied or use commenced until a Travel Plan Statement comprising immediate, continuing and long-term measures to promote and encourage alternatives to single-occupancy car use has been prepared, submitted to and approved in writing by the Local Planning Authority. The Approved Travel Plan Statement shall be implemented in accordance with the measures set out in therein.

Within three months of occupation, evidence of the implementation of the measures set out in the Travel Plan Statement has been submitted to and approved in writing by the Local Planning Authority unless alternative timescales are agreed in writing.

Reason: To support sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking and cycling.

29. Delivery & Servicing Plan

No building or use hereby permitted shall be occupied or use commenced until a delivery and servicing plan has been prepared, submitted to and approved in writing by the Local Planning Authority. The measures shall thereafter be implemented in accordance with the approved delivery and servicing plan for the lifetime of the development. The delivery and servicing plan shall include:

- a) The contact details of a suitably qualified co-ordinator;
- b) How vehicle arrivals, departures, parking, stopping and waiting will be controlled to minimise any impact on the adopted highway;
- c) Details of any freight consolidation operation, centre and the delivery and servicing booking and management systems;

Reason: In the interests of highway safety and to minimise the impact of vehicles servicing the development upon congestion

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30. Car Park Management Plan

No building or use hereby permitted shall be occupied or use commenced until a car park management plan setting out how the car park will be managed has been prepared, submitted to and approved in writing by the Local Planning Authority. The measures shall thereafter be implemented in accordance with the approved car park management plan for the lifetime of the development.

Reason: To ensure the safe operation of approved car park(s).

31. Waste Management Plan

No building or use hereby permitted shall be occupied or use commenced until a waste management plan setting out how waste will be stored and collected has been prepared, submitted to and approved in writing by the Local Planning Authority. The measures shall thereafter be implemented in accordance with the approved waste management plan for the lifetime of the development.

Reason: To ensure appropriate waste management facilities are provided to accommodate all waste generated by the development.

32. Reporting of Unexpected Contamination

In the event that contamination is found at any time that had not previously been identified when carrying out the approved development, it must be reported immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the Environment Agency's 'Land Contamination: risk management' guidance and BS 10175:2011 + A2:2017: Investigation of Potentially Contaminated Sites - Code of Practice. Where remediation is necessary a remediation scheme must be prepared which ensures the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Reason (for all conditions) : To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

Post occupation management

33. Restriction of parking level on site

Parking within the development site is to be restricted to the areas allocated on the approved plans and shall not encroach onto areas allocated on the plans for other uses.

Reason: To control the level of parking on the site and to safeguard the uses of other areas.

34. Protection of parking and servicing provision

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The areas allocated for vehicle parking, loading and unloading, circulation and manoeuvring on the approved plans shall only be used for the said purpose and not for any other purposes.

Reason: To ensure the provision and availability of satisfactory off-street parking and servicing/loading/unloading facilities for the development.

35. Single Occupancy - Student Accommodation Only

The development hereby approved shall be limited to single occupancy per room/studio flat.

Reason: In the interests of the residential amenity of future occupiers of the development hereby approved.

36. Employment Floorspace - Operating Hours, Class E(g)(ii) & (iii) Uses Only

Any use of the office floor space as a use defined as Class E(g)(ii) and (iii) shall not be carried out outside the hours of 08:00 to 18:00, Monday to Saturday. The building shall not be used on Sundays and Bank Holidays.

Reason: To safeguard the residential amenity of nearby occupiers.

37. Employment Floorspace - Operating Hours, Class E(g)(i)

Any use of the office floor space as a use defined as Class E(g)(i) shall not be carried out outside the hours of 07:00 to 21:00 Monday to Friday and 08:00 to 18:00 on Saturdays. The building shall not be used on Sundays and Bank Holidays.

Reason: To safeguard the residential amenity of nearby occupiers.

38. Use Restriction - Ground floor Office (E(g)(i,ii,iii) unit

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) the hereby approved Class E(g) unit shall only be used for uses permitted by their respective Use Class (E(g)) as defined by the Town and Country Planning (Use Classes) Order 1987 or any provision equivalent to that Class in any Statutory Instrument revoking and/or re-enacting that Order).

Reason: The employment floorspace is required to mitigate the loss of existing floorspace at the site, hence the unit must be retained as an Use Class E(g) use to meet relevant local plan policy.

39. Management Plan

The development hereby approved will be managed in strict accordance with the following section of the approved Premises Management Plan (Paragon Accommodation): 24 Hour Accommodation Management.

Reason: in the interests of amenity of neighbours.

Obscure Windows – North East Elevation, as labelled on plan

In accordance with the approved plans, those windows labelled as obscure fixed glazing will be retained as such for the lifetime of the development hereby approved.

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Reason: In the interests of the privacy of neighbours.

40. Obscure Windows - North East Elevation, as labelled on plan

In accordance with the approved plans, those windows labelled as obscure fixed glazing will be retained as such for the lifetime of the development hereby approved.

Reason: In the interests of the privacy of neighbours.

List of approved plans

41. List of Approved Plans and Drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision:

- BC00720, Drainage Strategy Report, rev. A, dated 21.09.2022, received 27.09.2022;
- 220922 - 157 West Street, Overheating Risk Assessment, R02, Mach Group energy, dated 22.09.2022, received 22.09.2022;
- Sustainability Statement and Energy Statement, rev. A, JMDC Services Ltd, dated 21.09.2022;
- 736351-1 (00) rev. 2, Desk Study and Preliminary Risk Assessment, Structural Soils Ltd, dated 07.02.2022, received 15.02.2022;
- Premises Management Plan (Paragon Accommodation), received 15.02.2022;
- 01, Travel Plan Statement, Transport Planning Associates, dated 09.02.2021, received 15.02.2022;
- BROADBAND CONNECTIVITY ASSESSMENT, date February 2022, CSJ Planning, received 25.02.2022;
- Arboricultural Method Statement, Bosky Trees, dated 24.05.2022, received 26.05.2022;
- 100 P1, Location and Block Plan, received 15.02.2022;
- 736351, Desk Study and Preliminary Risk Assessment
- 111 P6, Proposed Site and Roof Plan, received 16.09.2022;
- 112 P6, Proposed Elevations Sheet 1 of 2, received 16.09.2022
- 113 P5, Proposed Elevations Sheet 2 Of 2, received 16.09.2022;
- 114 P7, Proposed Ground Floor Plan, received 16.09.2022;
- 115 P6, Proposed First Floor Plan, received 16.09.2022;
- 116 P6, Proposed Second Floor Plan, received 16.09.2022;
- 119 P1, Proposed Sections Sheet 2, received 16.09.2022;
- 2112-030/TN/01, Technical Note 01: Response to Transport Development Management (BCC), July 2022, received 18.07.2022;
- 117 P2, Proposed Landscaping Plan, received 06.09.2022;
- 118 P1 Proposed Sections Sheet, received 02.11.2022

Reason: For the avoidance of doubt

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Advices

1 Works on the Public Highway

The development hereby approved includes the carrying out of work on the adopted highway. You are advised that before undertaking work on the adopted highway you must enter into a highway agreement under Section 278 of the Highways Act 1980 with the council, which would specify the works and the terms and conditions under which they are to be carried out.

Contact the Highway Authority's Transport Development Management Team at transportDM@bristol.gov.uk allowing sufficient time for the preparation and signing of the Agreement. You will be required to pay fees to cover the council's costs in undertaking the following actions:

- I. Drafting the Agreement
- II. A Monitoring Fee equivalent to 15% of the planning application fee
- III. Approving the highway details
- IV. Inspecting the highway works

NB: Planning permission is not permission to work in the highway. A Highway Agreement under Section 278 of the Highways Act 1980 must be completed, the bond secured and the Highway Authority's technical approval and inspection fees paid before any drawings will be considered and approved.

2 Traffic Regulation Order (TRO)

You are advised that a Traffic Regulation Order (TRO) is required. You must submit a plan to a scale of 1:1000 of an indicative scheme for a TRO, along with timescales for commencement and completion of the development. Please be aware that the statutory TRO process is not straightforward; involving the public advertisement of the proposal(s) and the resolution of any objections.

You should expect a minimum of six months to elapse between the Highway Authority's TRO Team confirming that it has all the information necessary to enable it to proceed and the TRO being advertised. You will not be permitted to implement the TRO measures until the TRO has been sealed, and we cannot always guarantee the outcome of the process.

We cannot begin the TRO process until the appropriate fee has been received. To arrange for a TRO to be processed contact the Highway Authority's Transport Development Management Team at transportdm@bristol.gov.uk

N.B. The cost of implementing any lining, signing or resurfacing required by the TRO is separate to the TRO fees, which solely cover the administration required to prepare, consult, amend and seal the TRO.

3 Impact on the highway network during construction

The development hereby approved and any associated highway works required, is likely to impact on the operation of the highway network during its construction (and any demolition required). You are advised to contact the Highway Authorities Network Management Team at traffic@bristol.gov.uk before undertaking any work, to discuss any temporary traffic management measures required, such as footway, Public Right of Way, carriageway closures or temporary parking restrictions a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary

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Traffic Management measures to be agreed.

4 Restriction of Parking Permits - Future Controlled Parking Zone/Residents Parking Scheme

You are advised that the Local Planning Authority has recommended to the Highways Authority that on the creation of any Controlled Parking Zone/Residents Parking Scheme area which includes the development, that the development shall be treated as car free / low-car and the occupiers are ineligible for resident parking permits as well as visitors parking permits if in a Residents Parking Scheme.

5 Highway Condition Survey

The development hereby approved includes the carrying out of a Highway Condition Survey. To agree the extent of the area to be surveyed contact the Highway Authority's Transport Development Management Team at transportDM@bristol.gov.uk

6 Excavation Works on the Adopted Highway

The development hereby approved includes the carrying out of excavation works on the adopted highway. You are advised that before undertaking any work on the adopted highway you will require a Section 171 (Excavation) Licence from the Highway Authority which is available at www.bristol.gov.uk/highwaylicences

7 Street Name and Numbering

You are advised that to ensure that all new properties and streets are registered with the emergency services, Land Registry, National Street Gazetteer and National Land and Property Gazetteer to enable them to be serviced and allow the occupants access to amenities including but not limited to; listing on the Electoral Register, delivery services, and a registered address on utility companies databases, details of the name and numbering of any new house(s) and/or flats/flat conversion(s) on existing and/or newly constructed streets must be submitted to the Highway Authority.

Any new street(s) and property naming/numbering must be agreed in accordance with the Councils Street Naming and Property Numbering Policy and all address allocations can only be issued under the Town Improvement Clauses Act 1847 (Section 64 & 65) and the Public Health Act 1925 (Section 17, 18 & 19). Please see www.bristol.gov.uk/registeraddress

8 Travel Plan Statement / Travel Plan - Not Submitted

You are advised that a Travel Plan Statement / Travel Plan is required to be prepared and submitted using the Travel Plan Guide for New Developments and the associated templates at www.bristol.gov.uk/travelplans

9 The Construction Environmental Management Plan should also include but is not limited to reference to the following:

o All works and ancillary operations which are audible at the site boundary, or at such other place

as may be agreed with the Local Planning Authority, shall be carried out only between the following hours: 08 00 Hours and 18 00 Hours on Mondays to Fridays and 08 00 and 13 00 Hours on Saturdays and at no time on Sundays and Bank Holidays.

o Mitigation measures as defined in BS 5528: Parts 1 and 2 : 2009 Noise and Vibration Control on

Construction and Open Sites shall be used to minimise noise disturbance from construction works.

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- o Procedures for emergency deviation of the agreed working hours.
- o Control measures for dust and other air-borne pollutants .
- o Measures for controlling the use of site lighting whether required for safe working or for security purposes.
- o Procedures for maintaining good public relations including complaint management, public consultation and liaison.

10. Sustainable Urban Drainage (SUDS)

The development hereby approved includes the construction/provision of a sustainable drainage system. You are advised to contact the Highway Authority's Flood Risk Management Team at flood.data@bristol.gov.uk before any works commence.

Supporting Documents

- 4.** 155-165 West Street
 1. Site Location & Block Plan
 2. Proposed Site & Roof Plan
 3. Proposed Elevations Sheet 1
 4. Proposed Elevations Sheet 2
 5. Proposed Ground Floor Plan
 6. Proposed First Floor Plan
 7. Proposed Second Floor Plan
 8. Proposed Landscaping



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Issued for Planning	11/02/2022	P1
REVISION SUMMARY	DATE	REVISION



First Floor, 43-45 Park Street
Bristol
BS1 5NL
mail@shuarchitects.uk
0117 248 2688

Project number
2060

Project
**155-165 West Street,
BS3 3PN**

Status
PLANNING

Use figured dimensions only. Only build from drawing marked for 'Construction'.

Drawing Title
Location and Block Plan

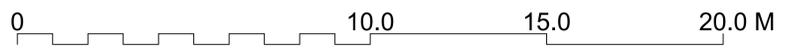
Drawn by
MP

Date
Feb 2022

Scale
1:500@A3

Drawing number
100

Revision
P1



Rooflight positions adjusted to reflect Block A staircase amendments	14/09/2022	P6
REVISION SUMMARY	DATE	REVISION



First Floor, 43-45 Park Street
Bristol
BS1 5NL
mail@shuarchitects.uk
0117 248 2688

Project number
2060

Project
155-165 West Street,
BS3 3PN

Status
PLANNING

Use figured dimensions only. Only build from drawing marked for 'Construction'.

Drawing Title
Proposed Site and Roof Plan

Drawn by
MP

Date
Jan 2021

Scale
1:100@A1

Drawing number
111

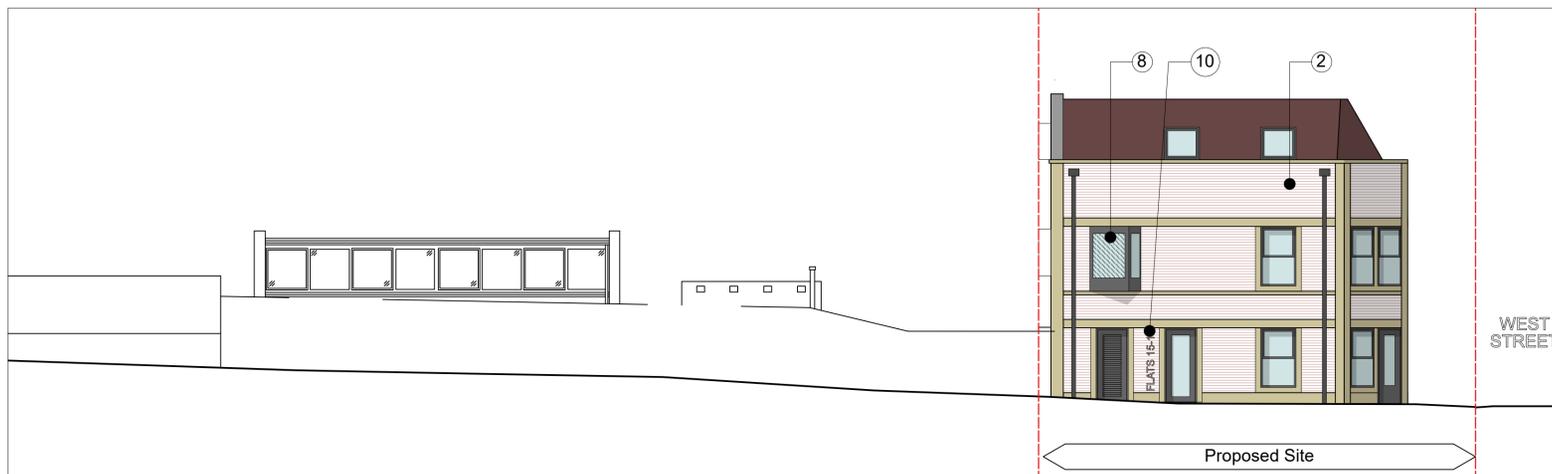
Revision
P6



A - SOUTH EAST ELEVATION : ALONG WEST STREET



B - NORTH EAST ELEVATION : ALONG CHAPEL BARTON

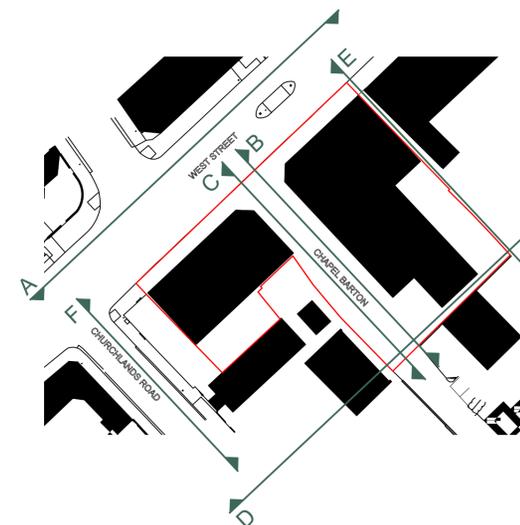


C - SOUTH WEST ELEVATION : ALONG CHAPEL BARTON

0 10.0 15.0 20.0 M

Notes

1. New signage for office
2. West Street Elevation: Red brick with coloured render banding, to wrap around elevation to Chapel Barton.
3. Cast stone coping
4. Aluminium hopper and downpipes. Colour: Dark grey
5. Triple glazed, aluminium framed openable rooflights. Colour: Dark grey
6. Red pantile roof
7. Triple glazed, aluminium framed openable window. Colour: Dark grey
8. Projecting slanted windows with opaque glass to front (hatched) and openable window to side.
9. Chapel Barton Elevation: Red brick with red brick corbel and corbeled banding to Ground Floor
10. New signage for student flat entrance
11. Red brick corbeled banding
12. Triple glazed, aluminium framed openable window with horizontal and vertical brise soleil. Colour: Dark grey window, dark red brise soleil
13. Projecting oriel window with opening windows to side. Triple glazed, aluminium framed, colour dark grey.
14. Vertical standing seam metal cladding. Colour: Dark grey
15. Louvred metal door. Colour: Dark grey



Elevation B - split window added to Block A stairwell. Provides a high level window to the ground-first floor landing and a low level window to the first-second floor landing.

14/09/2022 P6
REVISION SUMMARY DATE REVISION



0 10.0 15.0 20.0 M

Block A staircase amended, windows added
 REVISION SUMMARY
 14/09/2022
 DATE
 P7
 REVISION



First Floor, 43-45 Park Street
 Bristol
 BS1 5NL
 mail@shuarchitects.uk
 0117 248 2688

Project number
 2060
 Project
 155-165 West Street,
 BS3 3PN

Status
PLANNING
 Use figured dimensions only. Only build from drawing marked for 'Construction'.

Drawing Title
Proposed Ground Floor Plan

Drawn by
MP
 Date
Jan 2021

Scale
1:100@A1

Drawing number
114

Revision
P7



0 10.0 15.0 20.0 M

Block A staircase amended, windows added	14/09/2022	P6
REVISION SUMMARY	DATE	REVISION



First Floor, 43-45 Park Street
Bristol
BS1 5NL
mail@shuarchitects.uk
0117 248 2688

Project number
2060
Project
155-165 West Street,
BS3 3PN

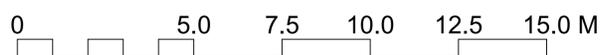
Status
PLANNING
Use figured dimensions only. Only build from drawing marked for 'Construction'.

Drawing Title
Proposed First Floor Plan

Drawn by
MP
Date
Jan 2021

Scale
1:100@A1
Drawing number
115

Revision
P6



Block A staircase amended, windows added	14/09/2022	P6
REVISION SUMMARY	DATE	REVISION



First Floor, 43-45 Park Street
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Project number 2060
Project 155-165 West Street,
BS3 3PN

Status
PLANNING
Use figured dimensions only. Only build from drawing marked for 'Construction'.

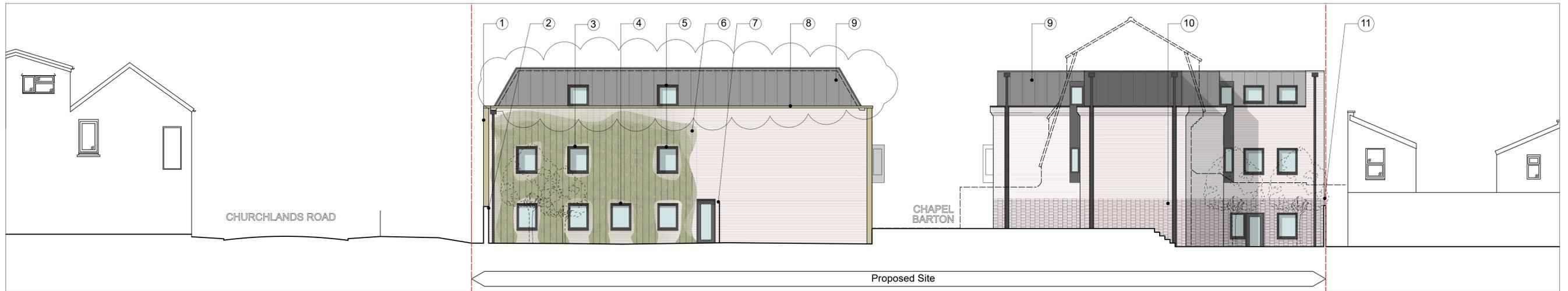
Drawing Title
Proposed Second Floor Plan

Drawn by
MP
Date
Jan 2021

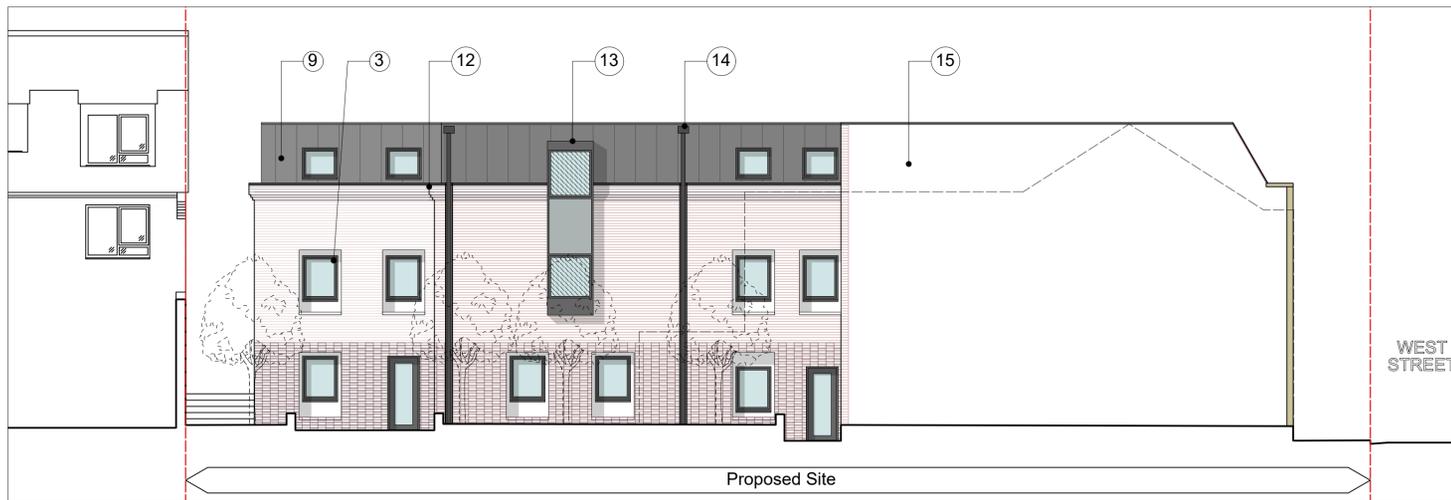
Scale
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Drawing number
116

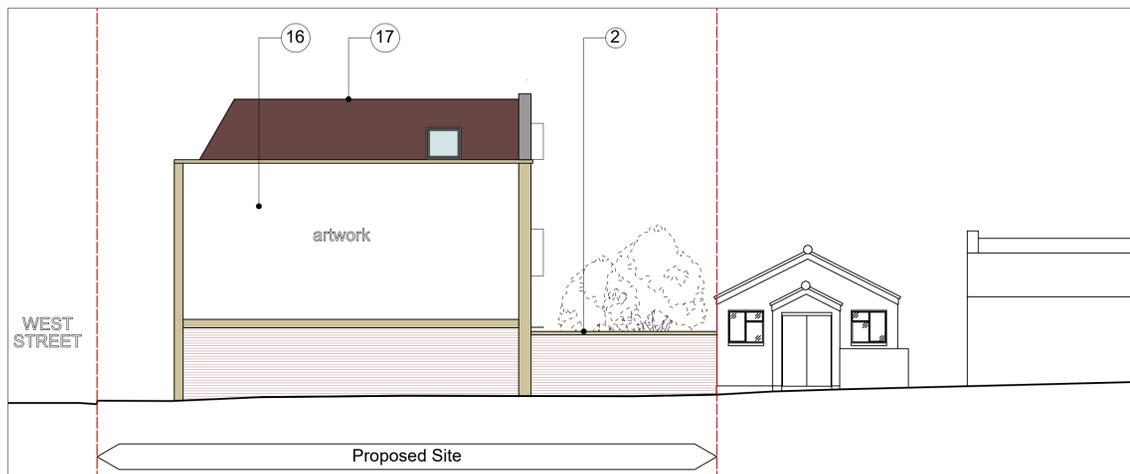
Revision
P6



D - NORTH WEST ELEVATION : REAR ELEVATION



E - SOUTH WEST ELEVATION : SIDE ELEVATION

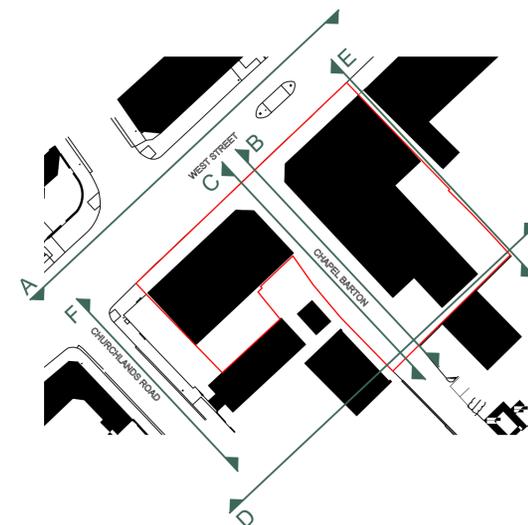


F - NORTH EAST ELEVATION : FROM CHURCHLANDS ROAD

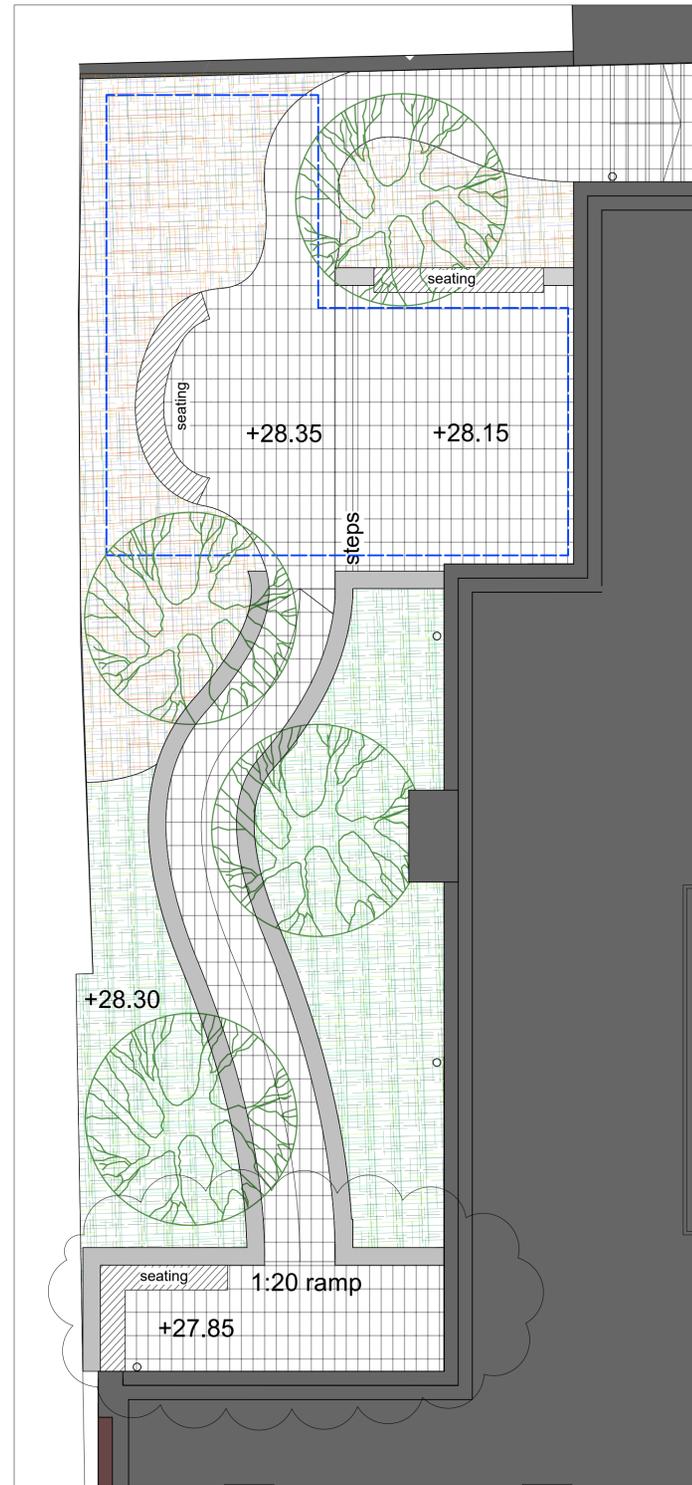
0 10.0 15.0 20.0 M

Notes

1. Coloured render banding
2. 1.8m high red brick wall with cast stone coping
3. Triple glazed, aluminium framed openable window. Colour: Dark grey
4. Horizontal perforated metal brise soleil to top of window. Colour: Dark red
5. Horizontal and vertical perforated metal brise soleil to top and side of window. Colour: Dark red
6. Climbers planted adjacent to building: Trachelospermum Jasminoides. To be supported by a Jakob Green Wall system of stainless steel ropes / mesh fixed to the wall.
7. 1.8m high vertical timber fence
8. Cast stone coping
9. Vertical standing seam metal cladding. Colour: Dark grey
10. Red brick corbeled banding
11. Existing vertical timber fence
12. Red brick corbel
13. Projecting oriel window with opaque glass to front (hatched) and clear, openable windows to side. Triple glazed, aluminium framed, colour dark grey.
14. Aluminium hopper and downpipes. Colour: Dark Grey
15. White render
16. Churchlands Road Elevation: Red brick to ground floor, coloured render banding with white render above. Proposed location for artwork to render.
17. Red pantile roof



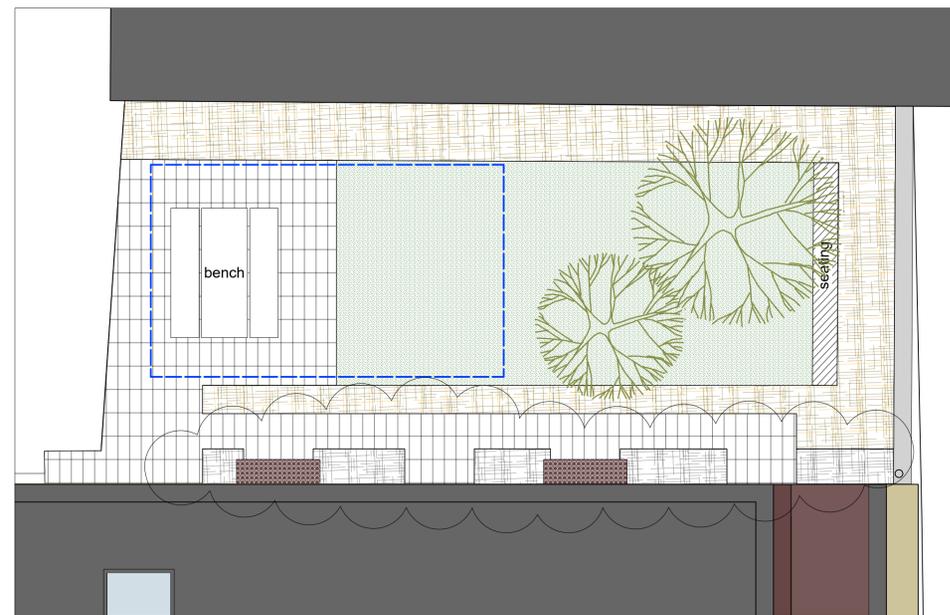
Elevation D - Upper section clad in standing seam metal	14/09/2022	P5
REVISION SUMMARY	DATE	REVISION



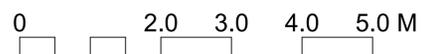
BLOCK A GARDEN

-  Proposed Acer Trees: 2 no. Acer Palmatum 'Sazanami', 2 no. Acer Griseum.
-  Proposed Amelanchier Trees: 1 no. Amelanchier alnifolia 'Northline', 1 no. Amelanchier 'Obelisk'.
-  Grasses border mix: Hakonechloa macra 'Aureola', Hakonechloa macra 'Nicholas', Carex oshimensis 'Evergold', Liriope muscari
-  Fern border mix: Asplenium scolopendrium, Athyrium niponicum var. pictum, Polygonatum odoratum 'Variegatum', Liriope muscari
-  Border mix: Ophiopogon planiscapus 'Nigrescens', Cornus alba 'sibirica', Erica carnea 'Myretoun Ruby'.

-  Climbers planted adjacent to building: Trachelospermum Jasminoides. To be supported by a Jakob Green Wall system of stainless steel ropes / mesh fixed to the wall.
-  Proposed turfed area
-  Permeable brick pavers laid in a jack-on-jack brick pattern with edging. Colour: Charcoal grey.
-  Integrated timber bench onto low level retaining wall or garden border
-  Cellular storage crates for drainage. Refer to Drainage Strategy Report prepared by Build Collective.



BLOCK B GARDEN



Climbers added. Additional garden seating added. Garden made accessible for wheelchair users.	02/09/2022	P2
REVISION SUMMARY	DATE	REVISION



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Project number 2060
Project 155-165 West Street,
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Status **PLANNING**
Use figured dimensions only. Only build from drawing marked for 'Construction'.

Drawing Title
Proposed Landscaping Plan

Drawn by **MP**
Date **May 2022**

Scale **1:50@A1**

Drawing number **117**

Revision **P2**